

Uniting Care Ageing

# Precinct Development Strategy

Precinct M – Eastern Residential / Seniors Living

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### 1.1 Introduction

#### 1.1.1 Intent

This Precinct Development Strategy (PDS) relates to Precincts M in the Shellharbour City Centre is prepared as required by the Shellharbour City Centre Development Control Plan 2001 (as amended) prior to lodgement of a Development Application for a site in excess of 5,000m<sup>2</sup>.

This Strategy, once approved by Council, gives both Council and Uniting Care a degree of certainty as to what will be developed in the future.

The PDS is required by the Shellharbour's City Centre DCP, and is generally a more detailed and parcel specific elaboration of the principles and controls contained within the City Centre DCP and the Master Plan.

Changing circumstances in the future may require review and possible amendment of this PDS. However, at the time of preparation it reflects Uniting Care's intentions in the context of Council's Masterplan/DCP and LEP.

Major variations to this PDS (once approved) will require justification in terms of how the intent, objectives and principles of the Masterplan are to be achieved."

#### 1.1.2 Community Need

Uniting Care has explored the opportunity to provide a mix of medium density housing types for seniors with some affordable and market housing, generally in mid-rise (3-4 storey) apartment form. These residential facilities will be supported by a range of community and commercial facilities appropriate to resident and surrounding community needs.

Also under consideration is a small local worship centre as well as a regional office for Uniting Care's community and social outreach programs.

This PDS applies to Precinct M which is a large undeveloped site between Wattle Road and Benson Basin.

### 1.1.3 Context

The Shellharbour City Centre has been in the planning stages for many years.

The early stages of development saw the creation of the Shellharbour regional shopping mall, a major big box retail centre containing supermarkets, a DDS, significant retail plus assorted services and council Administration Centre.

In 1998 Council carried out major Master Planning to facilitate the creation of a City Centre which would be a real place. This led to the creation of the Master Plan 1998, amended in 2002, 2005 and 2007 and the DCP adopted in 2001 and amended in 2003 and 2007.

The Shellharbour City Centre is intended as the major retail and community focus for the Southern Illawarra with catchment spreading from Wollongong in the north to Berry to the south with major catchment between Lake Illawarra in the north and Bombo in the south..



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### 1.1.4 Planning Framework

A variety of statutory and policy documents apply to these precincts. These include:

#### **The Illawarra Regional Environmental Strategy 2006-31**

The REP generally provides for a variety of uses without any particular constraints in the Shellharbour City Centre. The Regional Strategy identifies the Shellharbour City centre as a major regional centre. It supports a variety of uses as set out in the Master Plan and encourages medium to high density development in and around the centre.

#### **Existing LEP**

The current LEP (2000) permits mixed-use development, and medium density residential development on this and other city centre sites with no FSR or building height constraints.

#### **City Centre Master Plan**

The Master Plan identifies this site as a medium density residential site to support the City Centre. It was always intended that it would contain a major component of seniors housing.

The Master Plan is relatively silent on both FSR and on building height allowing considerable flexibility.

Early developments in the City Centre provided evidence for greater demand than envisaged at increased heights and FSR's.

The first round of sites were developed at 2-3 storeys and FSR's of up to 2.5:1. The second stage of sites are currently proposing 3-5 floors and FSR's up to 2-3:1.

The subject site, separated from low density residential areas by Wattle Road and adjacent to Regional Open Space and the Shellharbour Square Shopping Centre need not be constrained by either FSR or building height as it is quite clearly part of the City Centre.

#### **Shellharbour City Centre DCP (2008)**

The Development Control Plan aims to give form to the Master Plan. It proposed mixed-use development perhaps containing seniors housing with a building height of 3 storeys (or more with justification) without a FSR applying to the site.

## 1.2.0 The Shellharbour City Centre Frame

Councils City Centre Master Plan and Development Control Plan suggest that the frame of the Shellharbour CBD should support the proposed urban vitality of the city centre.

There should be medium/high density housing of diverse types for diverse markets and a variety of community support, mixed use and live-work uses.

The scale of development should be sensitive to the proposed scale of the CBD with high levels of pedestrian amenity provided by arcades, arcades, verandahs, shaded walks and footpaths, small squares and a strong relationship with the playing fields/town common.

Wattle Road is a major collector road accessing the City of Shellharbour. It is currently constructed as a two lane divided road. The high design standard combined with general low levels of traffic leads to excessive speeds.

Benson Avenue is a city access road and the lack of development along it causes some localised speeding.

New development should aim to increase friction and thus reduce speeding and therefore increase safety on these roads.



### 1.3.0 Eastern Residential Precinct

This edge of the city centre is proposed for a mix of seniors, affordable and conventional housing at medium/high density along with a range of open spaces, community and service uses. These will include:

#### Precinct M

- **Wattle Road** – density housing and community accommodation located between Wattle Road and Benson Basin suited to medium to high density housing. This will be integrated into the wider community and contain a number of connective open roads and pathways.

#### Precinct N

- **Benson Avenue** – this block will provide a range of medium density housing types suited to seniors whilst improving and enhancing connectivity within the Town Centre frame.

#### Precinct L

- **Town Common** (by others) – this adjacent precinct will comprise playing fields and recreational areas. Uniting Care will seek to draw amenity, provide access and integrate it with the residential edge of precinct M.



## 2.1 Design Intent - Precinct M

To provide medium density development integrating surrounding residential with the City Centre as well as providing residential and community support for the CBD.

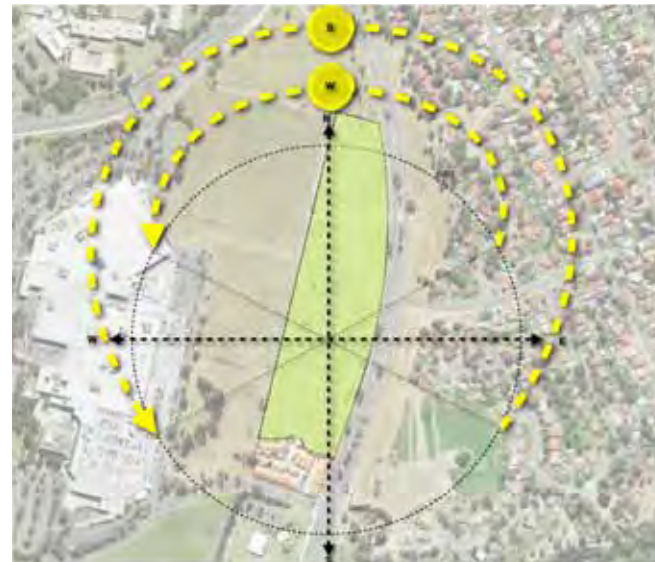
## 2.2 Issues

Major issues for this precinct include:

- A. Improving the streetscape of and interface with Wattle Road.
- B. In consultation with Council creating improved connection within the precinct and to the shopping centre.
- C. In consultation with Council, seek to facilitate an attractive, visually stimulating drainage system which enhances amenity of the area as well as providing detention and water quality functions.
- D. Improve and maintain existing pedestrian system through and around the site. In consultation with Council, improve the pedestrian connection between Barrack Heights and the Shellharbour city centre by addressing the pre-existing issues within the pedestrian tunnel under Wattle Road. Ensure pedestrian and resident safety through adoption of CPTED principles.
- E. Creation of density housing of various types to front (and share the amenity of) open space and also to front Wattle Rd.



Site Analysis - views



Solar Access



- F. Creation of community focus within the precinct to foster interaction between seniors, residents and the wider community.
- G. Provide high quality landscaping throughout the site.
- H. Provision of facilities, amenities and services to serve seniors and the wider community.
- I. Development of the site in an attractive and sustainable manner based on site characteristics including slope, solar access / microclimate, views / amenity and solar access.
- J. Ensuring that the height of new development complements the City Centre form.
- K. Provision of on-site and visitor parking to service development according to SEPP 65, Seniors Living SEPP, ARH SEPP and Councils City Centre DCP.



*Topography*

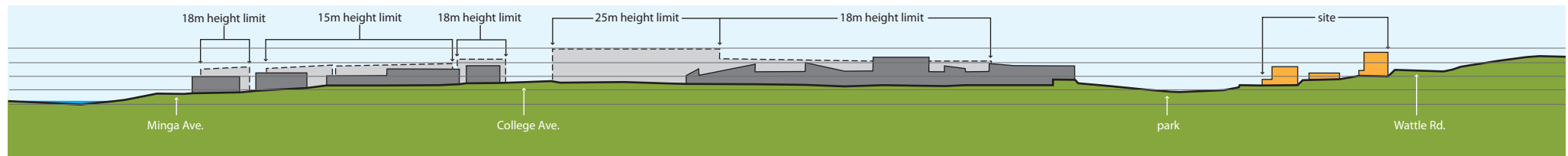


*Green Space*

L. Protection / Enhancement of Views and Vistas - Precinct M has little impact on views and vistas because:

- Nearby single storey buildings with high back fences open onto wide road reserve and open space providing little view vantage.
- Views to the west are of shopping centre.
- A small number of single storey dwellings have long views to Lake Illawarra. These views would be compromised with normal street planting in Wattle Road.
- Topography and open space set nearby buildings above and away from the site
- The plan and section to the right show significant sections and view impact assuming four storey buildings to Wattle Road. (See appendices for further discussion of views and vista in context of building height.)

M. Create safe paths of pedestrian movement which are direct, activated, stimulating, open and well lit.



## 2.3 Principles

### 2.3.1 Development Principles - Precinct M

- Create attractive building frontages and address points to Wattle Road.
- Improve and enhance street sections of Wattle Road with kerbside parking and strong avenue planting.
- Create an internal road/pathway system with connective, calmed streets.
- Provide opportunities for natural surveillance of the public domain.
- Ensure disability access throughout.
- Ensure pedestrian safety through adoption of CPTED principles.
- Provide a medium-high density housing precinct designed to universal design standards which provides a range of housing suitable for seniors.
- Provide affordable housing.
- Provide special needs housing to serve people with disabilities.
- Landscape and traffic calm Wattle Road and Benson Avenue where possible.
- Create a legible public domain which promotes wayfinding and an awareness of relative location, with clear transitions and boundaries between public and private space.
- Create *activated* public east-west and north-south connections for pedestrians.
- Create pedestrian and cycle systems throughout which are highly connective, *well-lit, with clear sightlines* and of high amenity.
- Address playing fields with density housing.
- Create an activity node within site (or on edge) for community interaction.
- Create parking for visitors on-site, as part of the edge road and parking mews.
- Create a strong built edge to the City Centre and a strong 'arrival point'.
- Contribute to the built form of the City Centre.
- Provide natural opportunities for surveillance of public domain.



Streetscape



Key Building Areas

- community/civic/commercial services
- vehicle links
- ⋯ pedestrian links

## 2.3.2 Movement

(see separate traffic study)

### (a) Connections

#### Vehicular

- Wattle Rd & Benson Ave are major local distributor roads.
- Traffic calming is desirable to reduce traffic speeds.
- Create calmed internal access roads .

#### Pedestrian

- Footpaths should be provided throughout.
- Traffic calmed environment should be pursued internally.
- Steps / ramps should connect Wattle Rd. with east / west paths through the site.
- Pathways should be formalised through the site,
- Pedestrian underpass should be connected through the site and on to shopping centre.
- Disability access should be provided throughout.

Note: In consultation with Housing NSW, the existing internal road (Davey Close) should be moved to the park edge because:

- Buildings will then front the road and the park.
- It can provide ready access to the park.
- It can provide a better distribution of public / private domain across the site with higher amenity and safer environment.
- Alternative and appropriate access can thus be provided to the rear of existing Public Housing.
- It provides improved casual surveillance over the adjoining Benson Basin open space.
- Seating should be used to identify a place or destination and reinforce activity points and the communal accessibility of the public domain.



Vehicle Links / Public Roads - existing



Primary Pedestrian Links

## (b) Street Types

(see Appendices for traffic report and proposed intersections / traffic calming)

### A. Avenue

- Wattle Rd. should include a parking lane in northerly direction (either on road as at present or indented into verge) to facilitate traffic calming and parking.

### B. Urban Road

- Benson Ave. should also be reconfigured to enable traffic calming including a pedestrian crossing point.

### C. Internal Roads

- Narrow streets with trees in pavement (2 parking & 2 moving lanes) sometimes with nose to kerb parking for visitor parking are proposed for East / West internal streets. These may be privately owned (under community title).

### D. Park Edge Road

- As above or may have nose to kerb parking for viewing locations and access to open space.



Circulation

— — vehicle links  
..... pedestrian links

## (b) Street Types (continued)

### A - Wattle Road

Wattle Street should be substantially traffic calmed in the following manner:

- one moving lane in each direction and one parking lane as existing (or two moving lanes and parking in verge)
- landscaped (major tree) buildouts into parking lane if possible
- trees planted in both verges and median
- footpaths constructed along verge
- Facilitate safe at-grade crossing of Wattle Road at the intersection with Carrington Street

### B - Benson Avenue

Benson Avenue should be reprofiled and calmed in the following manner:

- one moving lane in each direction
- one parking lane each side (if possible)
- trees also planted in verges
- footpaths on both sides

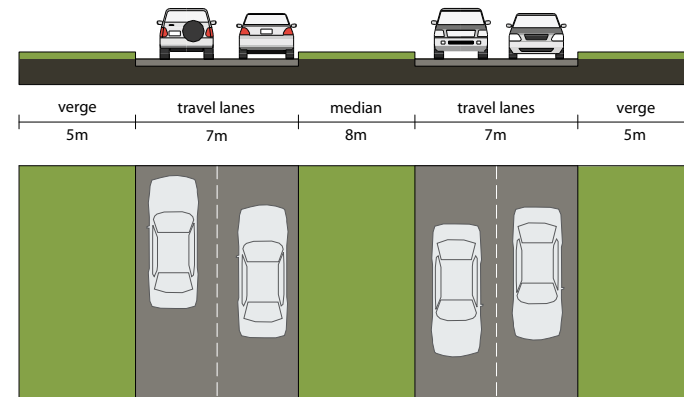
### C - Mews Roads

Mews Roads (desirably private) should be:

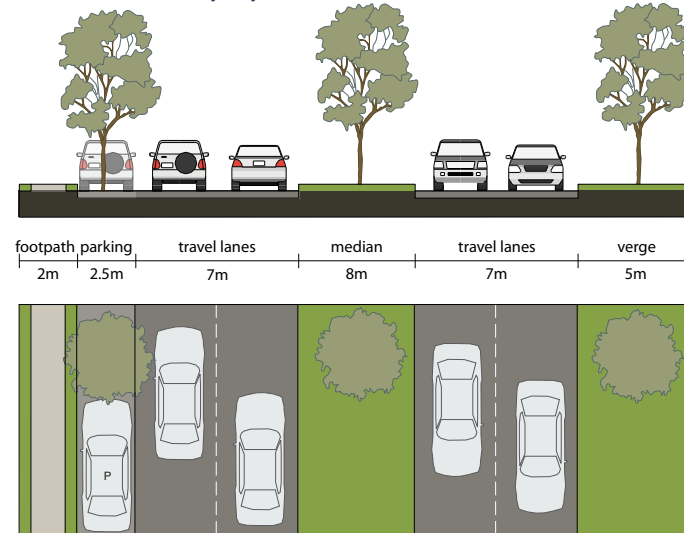
- traffic calmed
- include kerb side parking, perhaps some nose to kerb
- include all visitor parking
- contain a strong landscape theme
- provide safe footpaths

Note: The connecting mews to the pedestrian underpass must provide public access 24/7 and must be safe for pedestrians and cyclists.

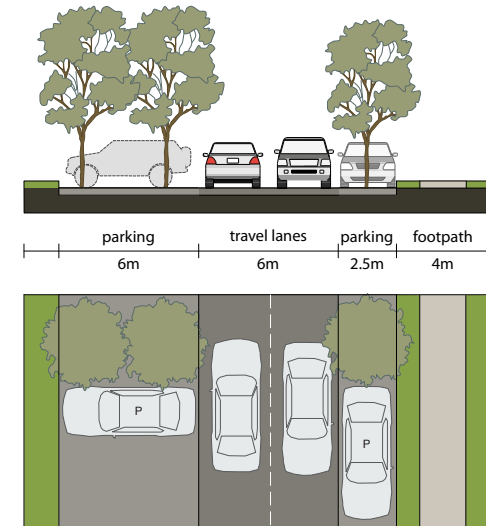
### Wattle Road - existing



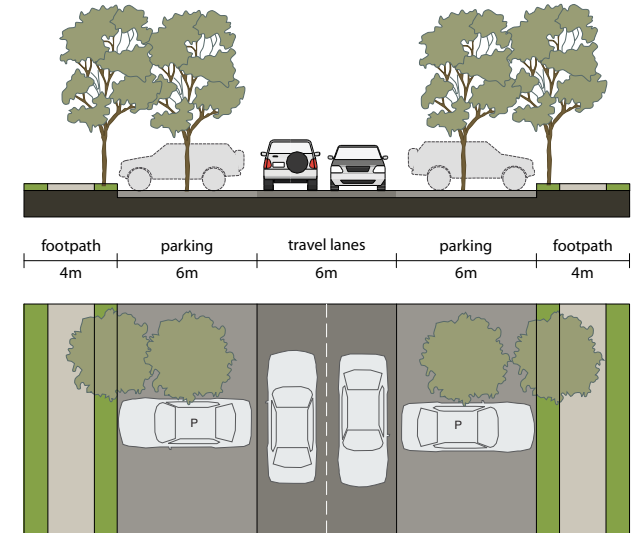
### A - Wattle Road - proposed



### D - Park Edge Street



### C - Mews



### (c) Movement - the pedestrian experience

Seek to create attractive and stimulating pedestrian experience moving through the site by considering the following elements:

#### A. Arbors

Animated pedestrian walk with enclosing vegetation.

#### B. Avenues

Strongly landscaped streets with landscaped footpaths.

#### C. Stairs

Continuation of streets to accommodate level changes.

#### D. Gardens

Pedestrian walks to continue through landscaped gardens (formal & informal).

#### E. Public Art & Landscape

A diversity of natural & man made experiences..

A. Arbors



B. Avenues



C. Stairs

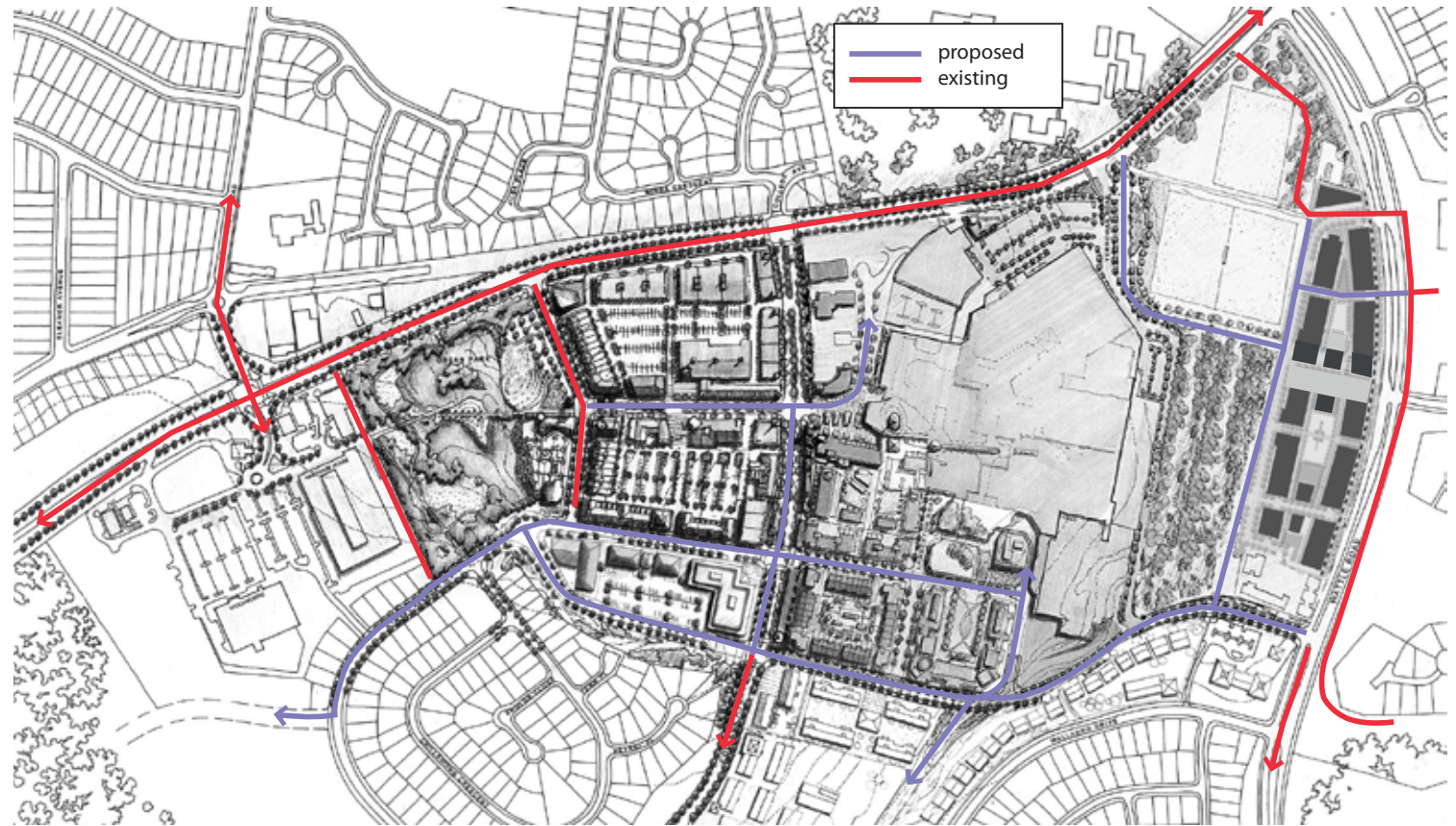


D. Gardens



### (d) Movement -bicycle paths

This precinct should connect into the existing subregional network most particularly an east-west connection should be provided and a north-south path along edge of open space.





### 2.3.3 Land Use

Land use will be predominantly residential with a significant components of seniors and affordable housing. Also included should be:

- Live/work
- Small scale commercial/employment uses
- Community and cultural facilities (including administration)
- Private and communal open space as set out in Master plan on this page.



Master Plan

### 2.3.4 Special Places

Placemaking is a very important element of community building. Elements of placemaking include:

- A walkable environment
- Safety and security
- Activities and interaction
- Spatial enclosure
- Excellent landscaping

The attached diagram shows the most appropriate locations for the creation of 'special places' which are further described on the following pages.

Illustrations are included to show how the above elements can achieve a genuine, memorable and enduring sense of place.



Placemaking Opportunities



## (a) Special Places

### A. E-W Lanes and Walks

This lightly trafficked lane and pathway system will improve E-W connectivity and connect the development (and residences to the east across the open space) with the existing entries to the shopping centre internal to the carpark.

The lanes will provide access to the community/housing development on this site, connecting via pedestrian stairs with Wattle Road and bring the community into social contact with the wider community.

### B. Stairs to Wattle Street

The steep grade across the site precludes regular road connections to Wattle Road. This does not however prevent pedestrian connection and pedestrian stairs are proposed at both E-W linkages. Stairs should be attractively designed and landscaped. Ramps and lift access should also be provided.

### C. Community

Community uses / resources will be provided for residents of the precinct and also for residents of the wider community.

### Principles

- Create beautiful parks, gardens, walks
- Create communal gathering places
- Provide seniors facilities that might have wider community uses
- Create community studios, workshops, meeting rooms

### Building Community



### Strong Avenue Planting



### Pedestrian Stairs



## (b) Shopping Mall Interface

### Principles (to be implemented by others)

The eastern face of the shopping centre carpark presents an unattractive development edge to the open space which is the major outlook for Precinct M. It would be desirable at some stage if its appearance could be enhanced.

- Clearly articulate eastern entry point to shopping centre (by others)
- Seek to screen blank walls and parking structures with:
  - > Screen walls
  - > Planting
  - > Possible cinema screen
  - > Active uses
- Create active footpath to access street

### Action

- Seek to negotiate with shopping centre owners to improve the interface with the open space and the view to the east.



Examples of 'treated' blank walls

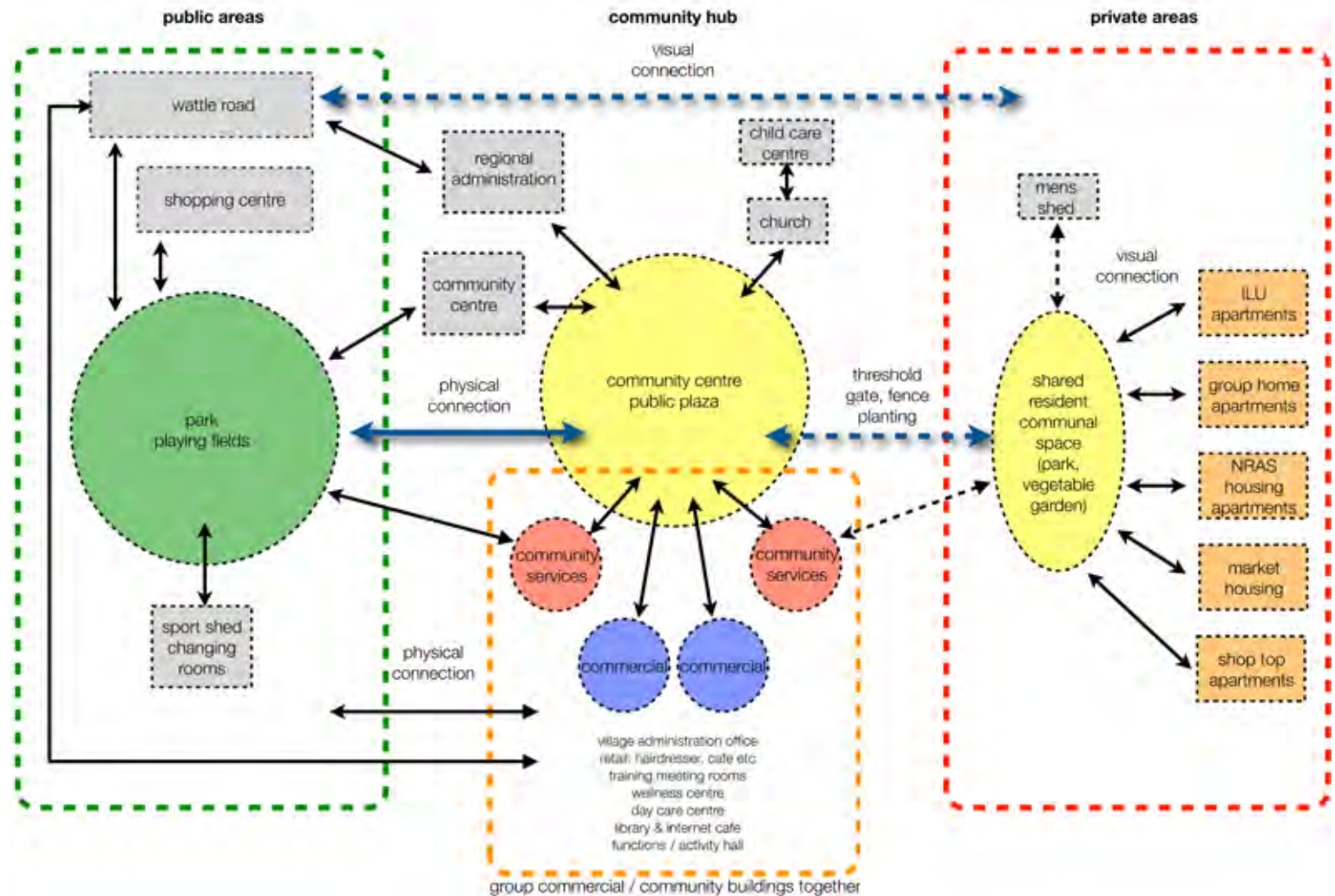
### 3.1 Design Interactions

The master planning of this precinct is complex and based on a very wide variety of land use and activity interactions.

The master plan must be very flexible so that changes to service needs requirements may be readily accommodated over time.

The attached flow chart attempts to describe the interrelated activities. They are broadly categorised as:

- Public Areas - true public domain which may be located off-site.
- Community Hub - on-site areas of engagement with internal and external community uses.
- Private Areas - which include communal (but non public) areas, private courts, yards, balconies and accommodation.



## 3.2 Precinct Master Plan

### 3.2.1 Urban Footprint

The figure to the right indicates the generally proposed building footprint comprising:

#### A. Wattle Road Buildings

Built up to 4 stories fronting Wattle Rd (and 5 stories internal to the site) and built with under building parking. These buildings should address Wattle Road with parking accessed internally from the lower level.

#### B. Park Edge Buildings

Built to 4 stories addressing the park edge road with under building parking (not visible from park edge road). This may contain some live / work, small commercial and / or community activities to the park edge road.

#### C. Community Buildings

Built to 1-3 stories providing communal activities for the development (perhaps with accommodation over), wider community activities, and some local commercial uses located to take advantage of passing pedestrian traffic.

#### D. Wider Community Buildings

Located on Wattle Road at the access road. This site is appropriate for a wider community use such as a church or a child care centre or perhaps regional administration.



Master Plan - urban footprint

### 3.2.2 Movement

The figure below indicates vehicular access and pedestrian / cycle movement through the site and connecting to the wider context.



Master Plan - movement

### 3.2.3 Community Uses / Activity Nodes

The figure below indicates desirable locations for community uses:

- A - wider community use (possible church, child care)
- B - internal community use



Master Plan - movement

### 3.2.4 Commercial / Community Activities

The figure below indicates appropriate locations for commercial and community uses which seek some level of engagement with the wider movement economy (Wattle Road or pedestrian path through the site).



Master Plan - Commercial / Community Activities

### 3.2.5 Public / Private Interface

The figure below indicates the manner in which the site can be organised into public domain, private domain and a communal domain (which permits controlled access by the general public).



Master Plan - Public / Private Interface



### 3.2.6 Community Access

The figure below indicates the proposed location for major community / social engagement between residents and the wider community.



Master Plan - Community Axis

### 3.2.7 Generalised Land Use

The figure below indicates notional land use distribution at ground level indicating:

- Residential to Wattle Road, park edge road, and internal courts
- Community uses in the most active locations
- Communal courts and gardens internally



Master Plan - Generalised Land Use

### 3.2.8 Parking

Parking for residents is proposed underbuilding using the slope to minimise unnecessary excavation. Parking for visitors will be provided on street (Wattle Road, Park Edge Road and Parking Mews on-site).

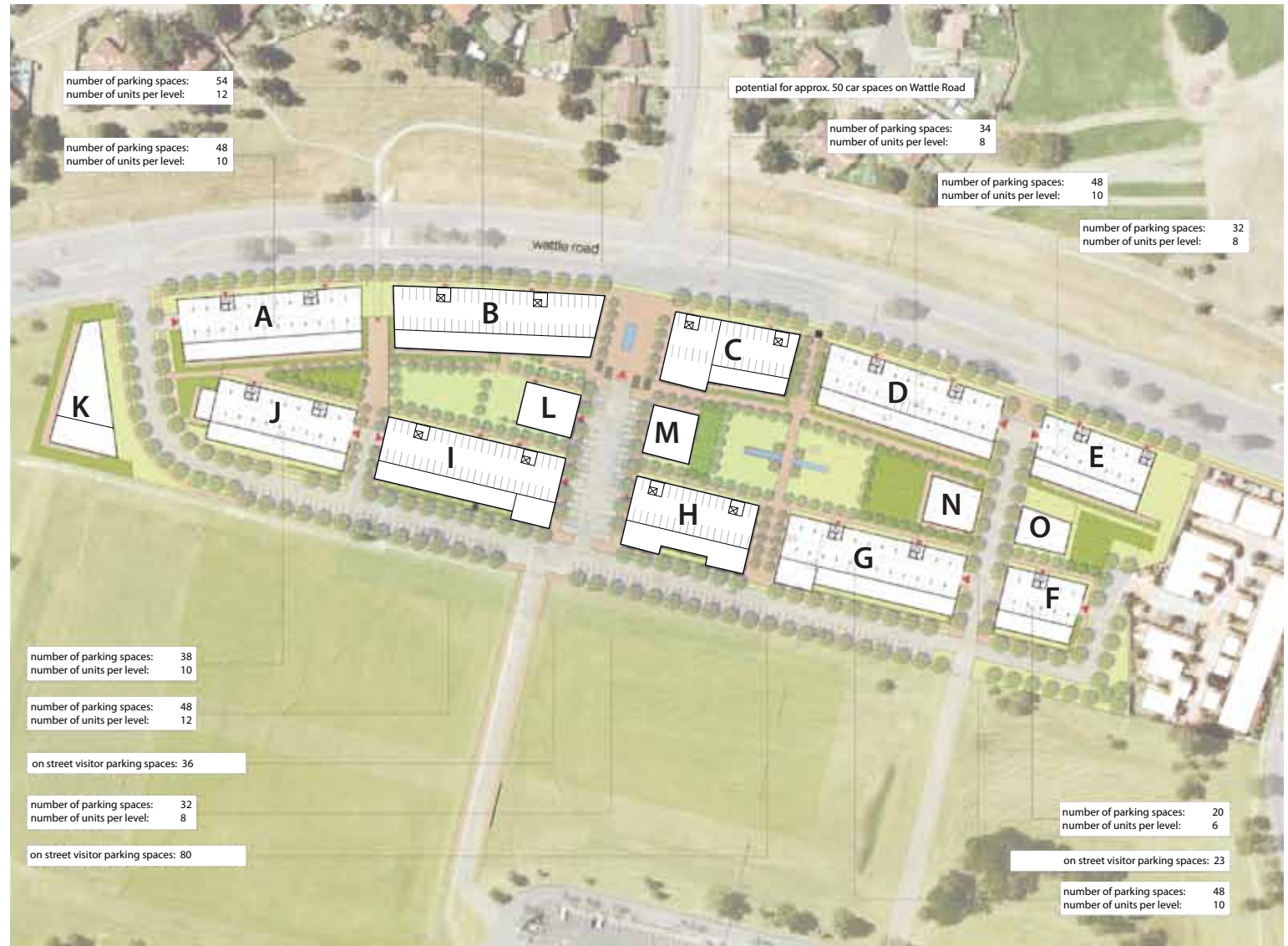
On-site underbuilding parking totals 420 spaces which is adequate for the development envisaged under the PDS and comprises the following:

A	48	I	48
B	54	J	36
C	34	K	20
D	48	L	--
E	32	M	--
F	20	N	--
G	48	O	--
H	32		
<b>Total Spaces</b>	<b>420</b>		

Visitors Parking comprises the following:

Wattle Street	50
Park Edge Street	80
Mews Parking	60
<b>Total Spaces</b>	<b>190</b>

This is adequate visitor parking for the mixed development envisaged by this PDS. (Note visitor parking includes commercial/retail/community and residential visitor parking.)



Parking

### 3.2.9 Waste Management

UnitingCare Ageing has undertaken a detailed audit of waste produced by its residents at similar developments within the greater Illawarra region. Based upon these findings, an estimate of waste production for the Shellharbour Precinct is as follows:

Waste Type:	Estimate of weekly volume per household:
Household Non-Recyclable:	3.2kg /16 Litres
Household Recyclable:	3.3kg / 11 Litres
Garden:	1.2kg / 1.7 Litres

The development will have dedicated storage and collection location points shown below and detailed in DA, using front lift, 3 cubic litre bins serviced weekly or fortnightly by a private bulk waste disposal service using the methodology detailed in Development Application (D. A.).



### 3.3.0 Building Types

A variety of density housing types may be considered for this precinct including low rise villas, townhouses and apartments. Live/work, community and retail services and facilities maybe incorporated.

#### 3.3.1 Apartments

Low (2-4 storey) apartments could be considered to take advantage of proximity to the centre, the environmental amenity and topography. Taller buildings should front Wattle Road and smaller buildings front the park edge road.



#### 3.3.2 Live / Work

Live / Work housing may be desirable to exploit the proximity to the centre and the ready accessibility. Live / work may include some social workshops or business incubator activities with residential above.



Whilst located throughout the site, Live / Work housing will be considered in buildings A, B, I and J (see figure p.24) in locations which improve surveillance over the east-west link and associated pedestrian tunnel.

### 3.4 Building Envelopes

#### 3.4.1 General Controls

##### FSR

Floor space ratio is not critical in this precinct. An overall FSR of approximately 1.1:1 over the whole precinct would seem appropriate with FSR for individual stages, perhaps up to 1.5.1.

##### Setbacks

Residential buildings should be setback 3-5m from front boundaries except where commercial or community uses might desirably front the footpath (setback 0m).

Side and rear setbacks may vary in the context of amenity, solar access and privacy.

##### Building Height

Buildings may vary between one and four to five storeys as indicated. Variation may be contemplated with justification.

##### Parking

On-site parking should be provided for all residential, community and commercial land uses to Council's City Centre DCP or State policy requirements.

Note:

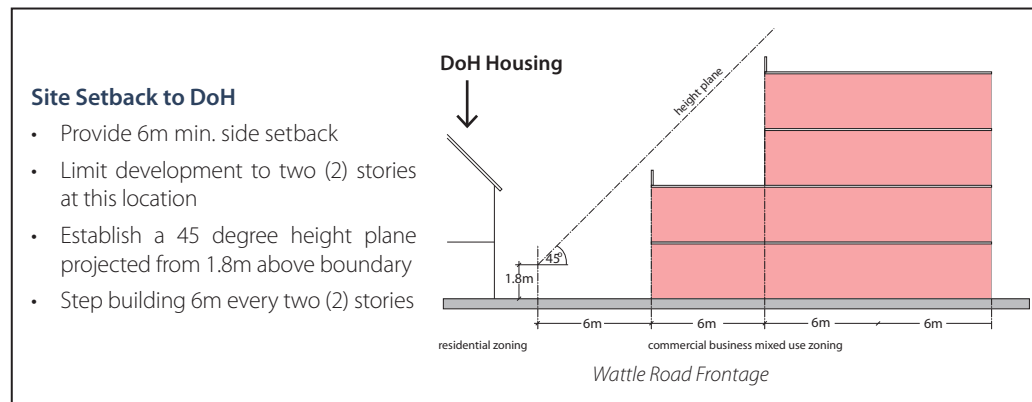
- Parking for seniors and affordable housing is regulated by State policy.
- Visitor parking should be provided where possible on-street.
- Parking for community services should be provided as required and as agreed with Council.
- Parking should be provided for 'normal' and not 'peak' events.



Notional Building Envelopes (from South West)



Notional Building Envelopes (from South West)



### 3.4.2 Design Concept

#### Park Edge Road

With 90 degree front-in parking and stepped bank facing oval.

#### Meeting Place

Commercial / Community hub. Comprises a shareway, community, commercial and live / work buildings around genuine public place.

Commercial / Community / live / work or incubator studios located here.

3-4 storey apartments to edge road. Mixed tenure and designed to universal design standards.

Community facilities for residents.



Master Plan

#### Community Building

Potential church, child care or regional Uniting Care offices.

Possible community gardens and open space.

Pedestrian Path connecting under Wattle Road, through the site to open space and mall.

Commercial node to Wattle Road.

4-5 storey apartments to Wattle Road. Mixed tenure and designed to universal design standards.

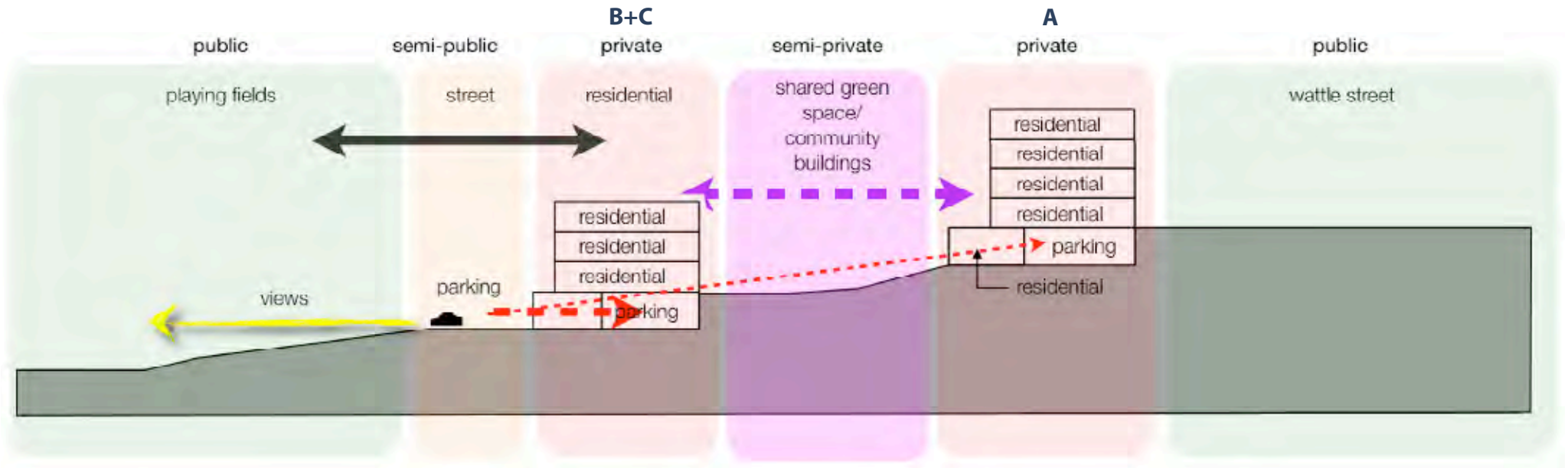
Wattle Road to be traffic calmed.

Existing Housing NSW to be integrated.

### 3.4.3 Using the Slope

The proposed development uses the slope as a series of benches whilst accommodating under building parking and providing ready access to parks and gardens.

A series of steps, ramps and elevators will be required to connect the various levels. The attached sectional diagram indicates the general concept.



### 3.4.4 Building Types & Details

#### TYPE A

##### Typology

Apartments 4 storey at Wattle Road with lifts and under building parking

##### A) Design Context / Integration

Buildings should front Wattle Road as major address and step down the slope towards Benson Basin using the slope.

They should enclose and engage with Wattle Road as the major entry into the City Centre.

##### B) Frontages

Buildings should be setback 5m from Wattle Road with approximately at grade pedestrian entry.

Building frontages should generally be built to a continuous setback line to avoid concealed entrapment areas.

Balconies, porches, entrances, bay windows may intrude into this 5m setback. (up to 2m)

##### C) Entrance / Access

Buildings should address Wattle Road (pedestrian and visitor address) with vehicle entry from lower level internal to the site.

##### d) Façade Treatment

Domestic nature with mix of warm brick . Other feature materials permitted.

Massing, proportion and detailing should be elegant.

##### e) Windows

Generally vertical in proportion or vertical subdivision and of timber or deep profile aluminium.

Verandahs, balconies, porches should be sheltered and part enclosed for climatic comfort.

##### f) Roofscape

Generally pitched >25 degrees but can contain some dramatic skillion elements.

Materials: tiled or metal deck, maybe flat and trafficable as decks/terraces.

##### g) Parking

Generally under building in basement/semi basement taking advantage of the slope.

Visitor parking should be on-street wherever possible.

##### h) Landscaping

The front side and rear setbacks of buildings should be generously landscaped to soften outlook and provide a garden setting for the building.

Communal open spaces shall be according to agreed landscape plan & may contain community vegetable gardens, lawns, bowling greens, parks, BBQ areas etc.

Front fences and dividing fences to public or quasi-public domain should be of semi transparent nature (timber or steel pickets, hedges, etc providing an appropriate measure of community engagement and privacy.

Transitions from private to public space must be clearly defined, both architecturally and by landscaping, domestic scale fencing and signposting .

Landscaping should maximise visibility and provide clear sightlines. Landscaping near

paths should not provide concealment locations or obscure the sightlines of pedestrians. Ground plantings associated with pathways should not exceed 0.5m in height. Planting will be designed so that views from dwellings are not obscured.

##### i) Safety and Security

All entries to buildings should clearly *address and be visible* from the street/pathways and be well lit. Buildings and streets should enable extensive passive surveillance from lobbies, windows, balconies, verandahs, etc. Shared entries should serve a minimum number of dwellings, with controlled access operated from within each dwelling. A clearly defined mail collection area is to be provided for each building.

##### j) Materials

All buildings should be predominantly of masonry construction to colour palette specification. Roofs should be tile or ribbed rolled steel (to specification) to colour palette. All elements should be constructed from sturdy, durable, vandal-resistant materials which can be easily repaired if damaged.

Surface treatments on high risk buildings, walls and fences should include graffiti resistant paints, materials or other surface treatments which discourage graffiti.

Verandahs, balconies, patios, decks, etc should be to approved specification.

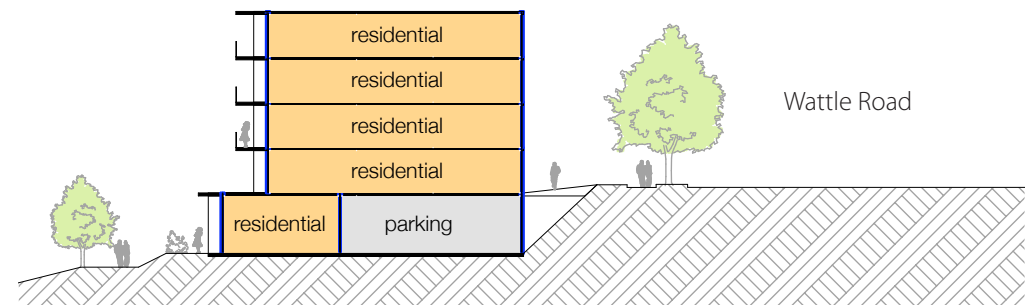
##### k. Car Parking

Entry to basement car parks will be controlled.

##### j. Lighting

Lighting will be used to direct pedestrians to safer routes at night. Pedestrians will not be encouraged to use the pedestrian tunnel after hours, due to the lack of surveillance on the opposite side of Wattle Road.

Areas which are not intended or required to be utilised at night will not generally be lit after hours, although ambient light from the surrounds would enable any illicit users to be identified from within other areas of the site.





## TYPE B

Apartment 3-4 storeys with under building parking and lifts.

### a) Design Context / Integration

Buildings should front service streets and open space areas (for passive surveillance).

### b) Frontages

Building should front service streets and overlook open space. 3-5m setback from street boundary.

Generous balconies, porches, bay windows, etc may intrude into setback. (up to 2m)

### c) Entry / Access

Pedestrian/visitors access should be provided from the service street where possible.

Vehicular access should ideally be from driveway (downhill side of building or building end).

### d) Façade Treatment

Domestic nature with mix of warm brick. Other feature materials permitted.

Massing, proportion and detailing should be elegant.

### e) Windows

Generally vertical in proportion or vertical subdivision and of timber or deep profile aluminium.

Verandahs, balconies, porches should be sheltered and part enclosed for climatic comfort.

### f) Roofscape

Generally pitched >25 degrees but can contain some dramatic elements. Materials tiled or metal deck as specified in colour palette or maybe flat and trafficable as decks/terraces.

### g) Parking

Generally under building in basement/semi-basement taking advantage of the slope.

Provision should be as per City Centre DCP or Seniors Living SEPP and / or ARH SEPP (if appropriate).

Visitor parking should be on-street wherever possible.

### h) Landscaping

The front side and rear setbacks of buildings should be generously landscaped to soften outlook and provide a garden setting for the building.

Communal open spaces shall be according to agreed landscape plan and may contain community vegetable gardens, lawns, bowling greens, parks, BBQ areas etc.

Front fences and dividing fences to public or quasi-public domain should be of semi transparent nature (timber or steel pickets, hedges, etc providing an appropriate measure of community engagement and privacy.

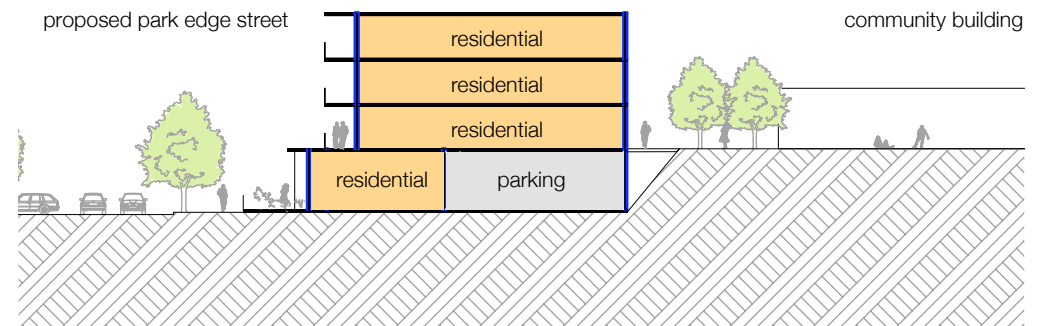
### i) Safety and Security

All entries to buildings should be clearly visible from the street. Entries (and streets) should enable extensive passive surveillance from windows, balconies, verandahs, etc.

### j) Materials

All buildings should be predominantly of masonry construction to colour palette. Roofs should be tile or ribbed rolled steel to colour palette.

Verandahs, balconies, patios, decks, etc should be to approved specification.



## TYPE C

A mix of apartment forms with some mixed use (local retail/community) at ground / street level.

### a) Design Context / Integration

Buildings should front park edge road. Community / retail uses should relate to the public edge / amenity.

### b) Frontages

Buildings should front edge road and overlook internal communal open space. 0-5m setback from street boundary.

Commercial, live / work, incubator, studio space should be separately accessed from front courtyard and from public street.

Balconies, porches, bay windows, etc may intrude into setback (up to 2m).

### c) Entry / Access

Pedestrian/visitors access from street.

Vehicular access from service roads to under building parking.

### d) Façade Treatment

Domestic nature with mix of warm brick. Other feature materials permitted.

Massing, proportion and detailing should be elegant.

Verandahs, balconies, porches should be sheltered and part enclosed for climatic comfort.

### e) Windows

Generally vertical in proportion or vertical proportioned subdivision. Desirably timber or deep profile aluminium.

### f) Roofscape

Generally pitched >25 degrees but can contain local variation or emphasis.

Materials tiled or metal deck as specified in colour palette or maybe flat and trafficable as decks/terraces.

### g) Parking

Generally under building in basement/semi basement but not visible from edge road.

Provision should be as per City Centre DCP or Seniors Living SEPP and / or ARH SEPP (if appropriate).

Visitor parking on-street.

### h) Landscaping

The front side and rear setbacks of buildings should be generously landscaped to soften outlook and building appearance.

Front setback landscape should integrate with street landscaping.

Common open space areas should be landscaped according to landscape plan and may include community gardens, bowling greens, community buildings/community rooms, indoor pool, clubhouse, parks, BBQ areas, etc.

Front fences, hedges and dividing fences should generally be transparent (timber or steel pickets, hedges, or similar) with appropriate balance between street engagement and privacy. Front fences may be removed where commercial / community use is provided.

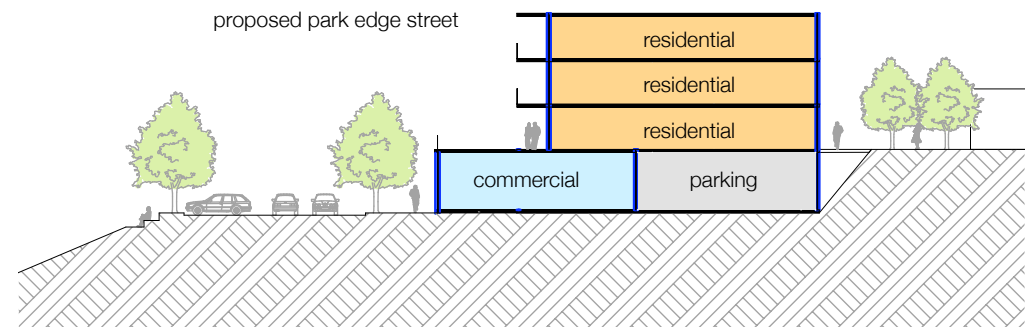
### i) Safety and Security

All entries to buildings should be clearly visible from the street. Entries (and streets) should enable extensive passive surveillance from windows, balconies, verandahs, etc.

### j) Materials

All buildings should be predominantly of masonry construction to colour palette specification. Roofs should be tile or metal deck (preferably ribbed rolled steel) to approved palette specification).

Verandahs, balconies, porches, etc should be submitted with and DA.



### 3.5 Public Domain Works

#### 3.5.1 Pedestrian Underpass / Parking Mews / Through Site Link

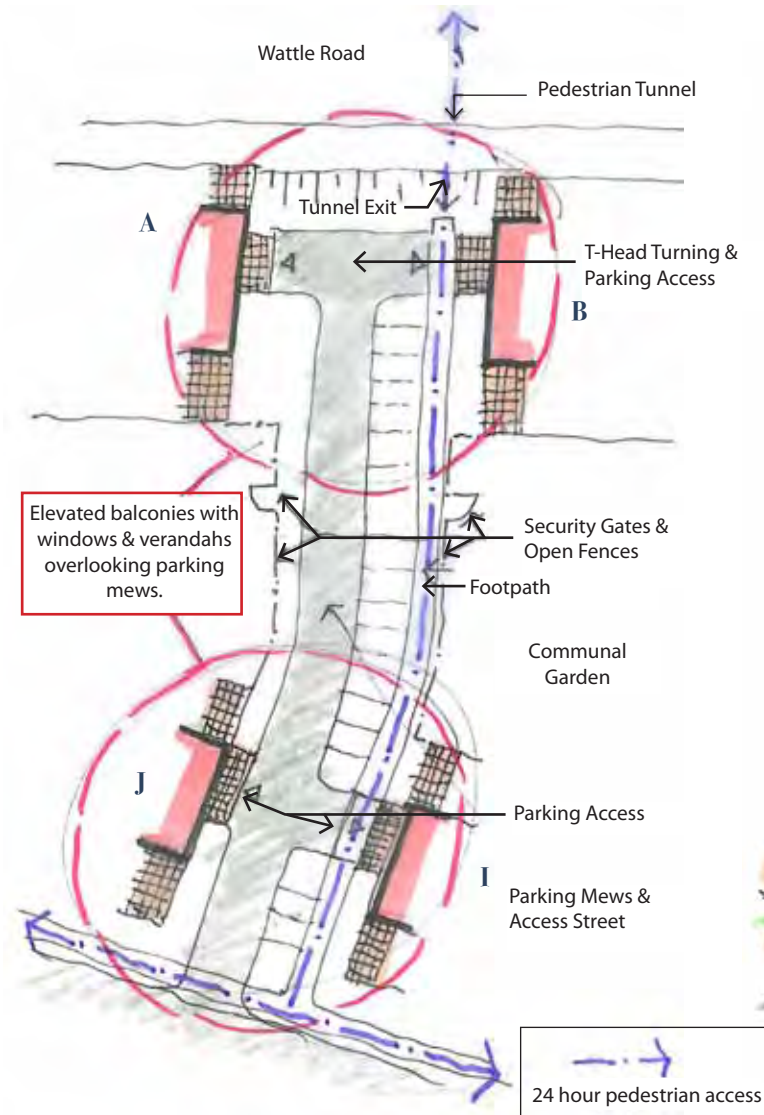
The underpass to Wattle Road connects with an internal street / parking mews (visitor parking).

This mews is separated from communal gardens by an open steel picket fence and has strong public surveillance from a number of balconies and terraces which overlook the street.

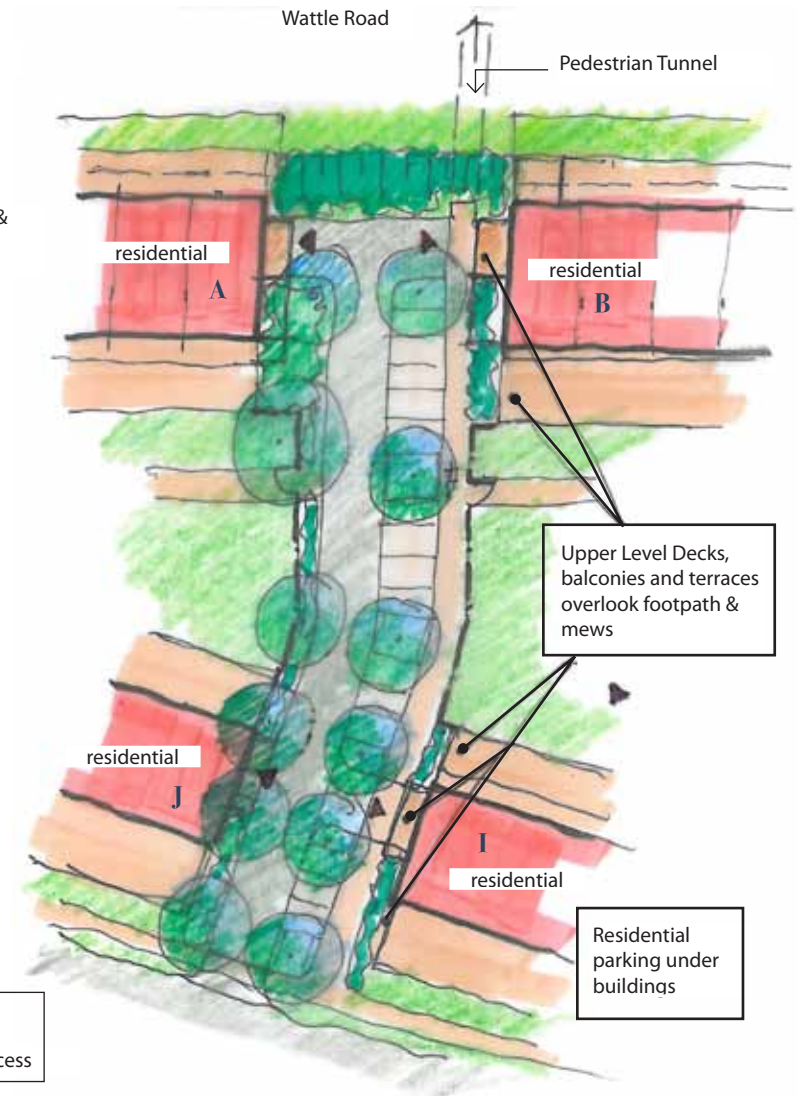
The street provides access to secure under building / basement parking for dwellings as shown.

The public domain applies strong CPTED principles viz:

- There are no hiding places / places of concealment.
- The public domain is overlooked from a number of windows, balconies and terraces.
- There is clear definition between public domain and private / communal land (with appropriate security precautions).
- Building frontages built to a continuous setback line to avoid concealed entrapment areas.
- Live / Work housing may be considered in buildings A, B, I and J in locations which improve surveillance over the east-west link and pedestrian tunnel entry.
- Landscaping at the tunnel entry will be limited to low groundcovers. Bollards will prevent vehicular access into the tunnel.



Detail A - diagram



Detail A - concept

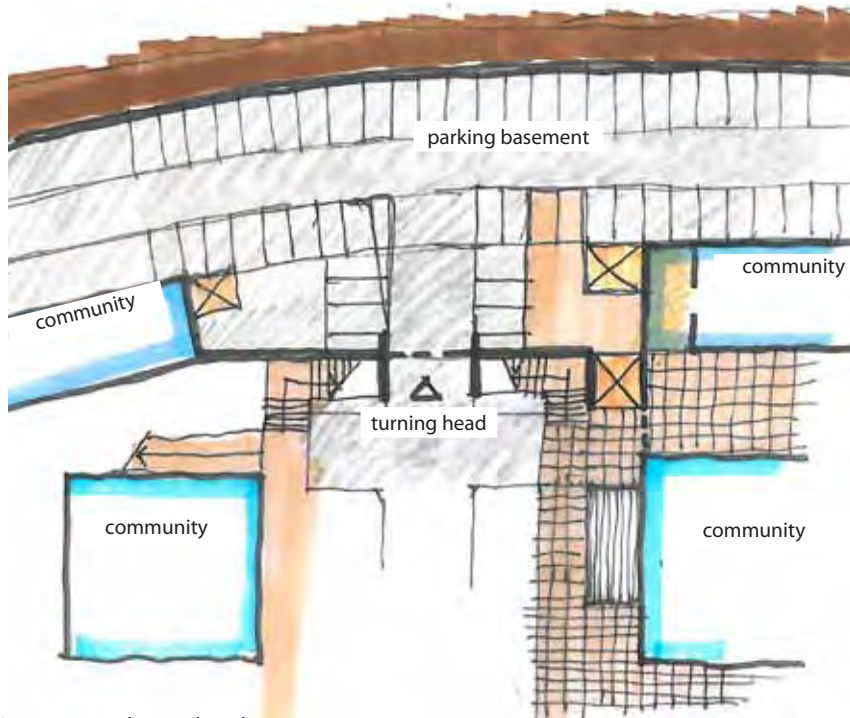
### 3.5.2 Public Square / Parking Mews / Through Site Link

A public square is created at the intersection of Carrington Street. Pedestrian access is provided to the lower level by stairs, ramp and elevator.

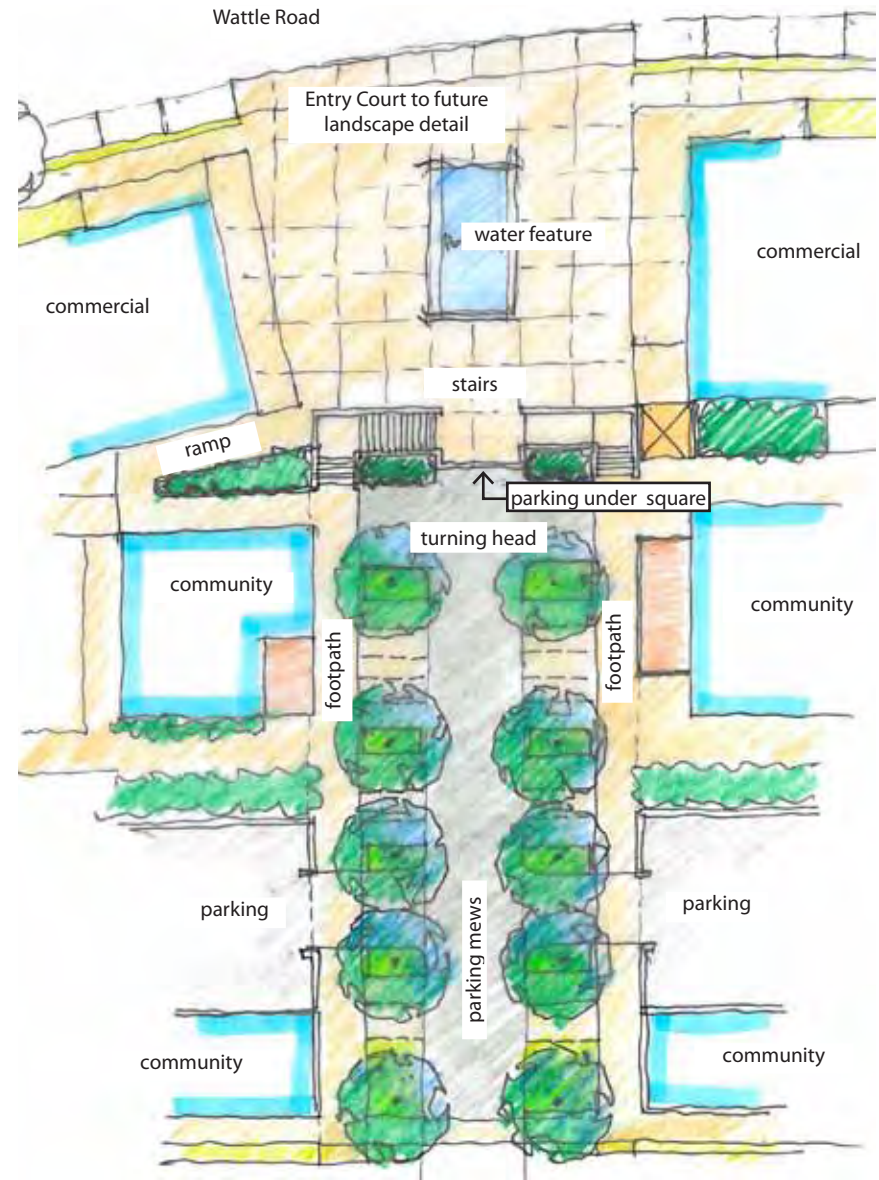
Visitor parking is provided (nose to kerb) in the parking mews with avenue planting. Commercial / community uses line the public domain. Parking is under building and under the pedestrian square.

Excellent public surveillance is provided from commercial / community buildings and from residential dwellings. Communal gardens etc. are gated behind visually permeable steel picket fences.

As with 3.5.1 CPTED principles apply particularly with respect to limiting opportunities for concealment to strong passive surveillance and to clear definition of public / and private / communal domain.



Detail B - concept lower level



Detail B - concept

# Appendices

- A. Variations to Master Plan**
  - 1. Introduction
  - 2. The Proposal
  - 3. Consistency With the Master Plan
  - 4. City Centre Form

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## A. Variations to Master Plan

### 1. Introduction

This Precinct Development Strategy (PDS) is intended to indicate how the development of a particular site or precinct will contribute to the implementation of the Shellharbour City Centre Master Plan (The Master Plan) and Shellharbour City Centre DCP (The DCP).

The City Centre DCP suggests that height, and building envelopes/footprints, setbacks in PDSs should accord with the Master Plan. However the DCP provides that:

“In exceptional circumstances, where it can be shown to Council’s satisfaction that, to achieve the objectives set out throughout this PDS and The Master Plan, it is unnecessary and unreasonable for the full provisions or standards set out in this Plan to be met, Council may vary one or more of the requirements for particular developments. In this respect the Development Application (DA) or Block or Precinct Development Strategy (PDS) will need to demonstrate how the relevant objectives are otherwise met.”

There is a reference in The Master Plan to maximum residential building heights.

This appendix provides justification for the variation to the height controls in the DCP.

A number of things have changed since the production of the original Master Plan in 1998, and indeed, it has been amended a number of times to accommodate these changes. The most recent being 2007, when the Master Plan was amended to accommodate Stockland’s latest development intentions (now under construction). In this case the PDS process with Stockland led Council to make changes to The Master Plan (and the DCP). No changes were required to the LEP.

The Master Plan identifies Precincts M&N as medium density mixed-use precincts desirably containing seniors housing. The original Master Plan provided two site options for Precinct M (as Landcom/Council were still negotiating open space dedication).

We now have a defined site (previous Option 2), owned by UnitingCare who wish to develop it as a mixed-use medium density precinct with a predominance of seniors living but also containing affordable housing, special housing and community facilities.

This is precisely what was proposed in the Master Plan.

### 2. The Proposal

The proposal seeks to provide a range of socially productive uses across the site including seniors, affordable and special housing, community and social services for the residents and wider community, administrative facilities and some local retail to service the site and wider community.

In this context Uniting Care seeks the flexibility for a wide range of community and commercial uses and a diversity of housing types.

A building height of 4 storeys to Wattle Road (this will in effect be 5 storeys at the rear when accounting for the slope) and 4 storeys to the proposed new park edge road is proposed.

This is comparable with the rest of the City Centre where 3-5 storeys has been permitted (including Council site at 4-6 storeys and Stockland at 5 storeys) with FSR’s (not seen as the prime driver) up to 2:1 in some sites e.g. Lots 2 and 4 Stage 2).

### 3. Consistency with the Master Plan

The Master Plan provides guidance and direction for the future development of the city centre. It provides for residential uses in areas of high amenity and on the edges of the City Centre. These could, in many cases, contain mixed uses with retail, entertainment, commercial or home office at ground level and residential above.

This plan (the Master Plan) illustrates a general height limit of 3 storeys with potential for an extra floor in significant locations and where the urban form of the City may be celebrated. The Master Plan provides that precise heights and envelopes will need to be justified in Precinct Development Strategies.

Section 3.2 of the Master Plan sets out the planning and development principles. The development envisaged under this PDS is consistent with these principles as indicated in the following table.

Table 1 Master Plan - Planning and Development Principles

Principle	Consistency
Establish strong connections with existing retail/commercial development.	Strong connections through the site to the retail centre are maintained and enhanced.
Create a flexible framework for future growth.	The development proposal is flexible to change as housing needs change and evolve.
Enhance open space and drainage areas as valuable public domain elements.	The development complements the potential open space uses in Bensons Basin.
Create strong and attractive public domain.	A high quality public domain within the site is proposed including streets and walkways which integrate with surrounding areas.
Seek appropriate sites for future civic and entertainment precincts.	Not relevant
Encourage a diversity of commercial, retail, entertainment and civic uses	A diversity of associated community and commercial uses compatible with a residential environment are proposed.
Encourage mixed use development (retail/commercial, retail/residential) where possible to improve vitality and viability of centre.	Mixed use development is proposed.
Ensure ready access and circulation of public transport.	The development envisaged under the PDS will support further public transport provision.
Ensure adequate parking with potential for some future structured/underground parking.	Parking is to be provided in accordance with City Centre DCP and other environmental planning instrument requirements.
Create public places, civic focus and people places taking advantage of short and long views and proximity to open space/drainage features.	Public access through the site is proposed connecting with the footpath system to the east and west of the site. Views are enhanced to and through the site.
Link Stockland Shellharbour Shopping Centre and Memorial Drive with a public space that is of appropriate scale and has a sense of enclosure surrounded by active uses.	Not relevant to this precinct.
Develop a central spine to focus activity with distinct public spaces along its length.	Not relevant to this precinct.
Facilitate the creation of a vibrant, active, safe and pedestrian orientated main street.	Not relevant to this precinct.
Improve levels of integration for pedestrians, cyclists, bus users and motorists and clearly define and contain the City Centre edge.	The development envisaged under the PDS provides a strong residential edge to the City Centre reinforcing the road network and integrating pedestrian and cycle connections with surrounding areas.
Respond to realistic market demand in staging.	The development will be staged to respond to market demand.
Ensure attractive edge treatment to New Lake Entrance Road.	Not relevant to this precinct.
Create a highly visible and urban character so that the centre is easily recognisable.	The development envisaged under the PDS provides a visible urban character appropriate in a city centre location and will reinforce the centre as a place of activity and city centre character.
Encourage a modern and contemporary architectural style, high standard and quality building design in the City Centre.	The PDS envisages a modern and contemporary architectural style.
Work towards ensuring an appropriate balance between economic, social and environmental issues.	The development will provide an excellent social outcome by providing housing for a range of special needs including aged and affordable housing in a highly accessible city centre location.
Ensure good levels of visual permeability & pedestrian connectivity within & between precincts.	The layout provides for visual permeability between buildings to the city centre and maintains pedestrian connections to the city centre.

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The Master Plan sets out the following design objectives for this precinct:

- Provide for future residential development including seniors living/ seniors facilities:
  - > two and three storey residential apartments;
  - > a mix of housing types and styles within a co-ordinated framework (possibly retirement living);
  - > extensive street planting on edges and continuation of avenue planting along Wattle Road;
  - > the provision of a limited palette of front fence types to contribute to the character of the public domain;
- Development to front both Wattle Road and internal access road;
- On-site parking for residential development
- Provide east-west thru-site pedestrian/ cycle connection with potential eastern access to Stockland Shellharbour;
- Vehicular access to be determined as part of Benson Basin Master Plan which will stem from section 94 review.

The development envisaged under the PDS is totally consistent with, and incorporates, these design objectives with a higher building form. This higher building form is considered acceptable and justifiable for the following reasons;

- It represents an appropriate density of development in a city centre location and is consistent with regional planning strategies in this regard;
- It provides a clearly defined and strong edge to the City Centre consistent with the Master Plan planning principles;
- It facilitates the provision of housing and associated uses by a community housing provider;
- Traffic generation can be accommodated on the surrounding road network;
- Building height and intensity is such that parking can be accommodated on site;
- It is compatible with the height and scale of buildings in the City Centre and will remain subservient to the higher built forms permissible in the City Centre;
- It has no significant adverse impacts on adjoining or adjacent development.

Thus 4 storeys above street level is an appropriate height limit for the site. This can be compared with the proposed 15 metre and 18 metre height limits proposed for the City Centre to the west of the site.

This height limit should be measured at the adjacent streets and permit an additional part floor to sleeve under building parking areas for sites fronting Wattle Road.

A 4 storey height limit to Wattle Road is appropriate given the wide (30m plus) reservation with median and the fact that this is a major entry into the City Centre. This would relate to a building height of 15m to Wattle Road and 18m internally.

Four storey buildings along the park edge is readily justified by the fact that units can draw amenity from the park and they are separated from the park by a wide edge road which provides visitor parking and strong passive surveillance to the playing fields / open space. Note that the lower floor (residence / commercial / community use) represents a shallow 'sleeve' to under building parking which can 'activate' and survey the street. This would relate to an internal height of 9-12m and a Park Edge Road of 15m.

The proposed development with a number of 4 storey buildings fronting Wattle Road will change the visual framework of this area. However it is considered appropriate in the context of the allowable building heights in the City Centre and the height of the ridge to the east of the site with building height on the site remaining generally subservient to these building forms and natural features.



#### 4. City Centre Form

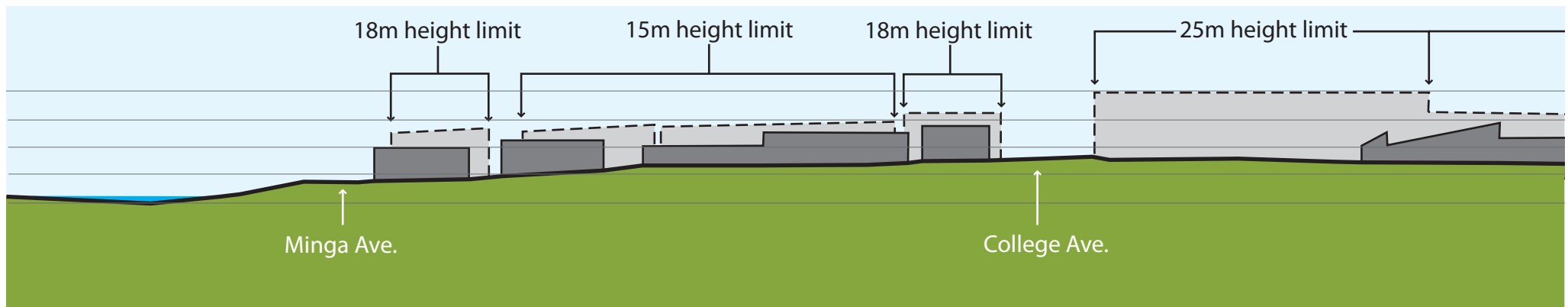
The City Centre extends in an East-West direction from Wattle Road across and through the subject site (4 stories and 15m at Wattle Road and Park Edge Road frontages), across Benson Basin open space then through the shopping centre (approximately 10m but 15m permissible) to the town square where a 25m (7-8 storey) residential-mixed use building is permissible. Mixed use buildings of 18m (5 storeys plus roof) are permissible on College Avenue corners of Memorial Avenue. Buildings must then step down Memorial Avenue at 15m (4 storeys and roof) to 18m (5 storeys and roof) at the intersection of Memorial Drive and Minga Avenue and then on into the town park / wetland.

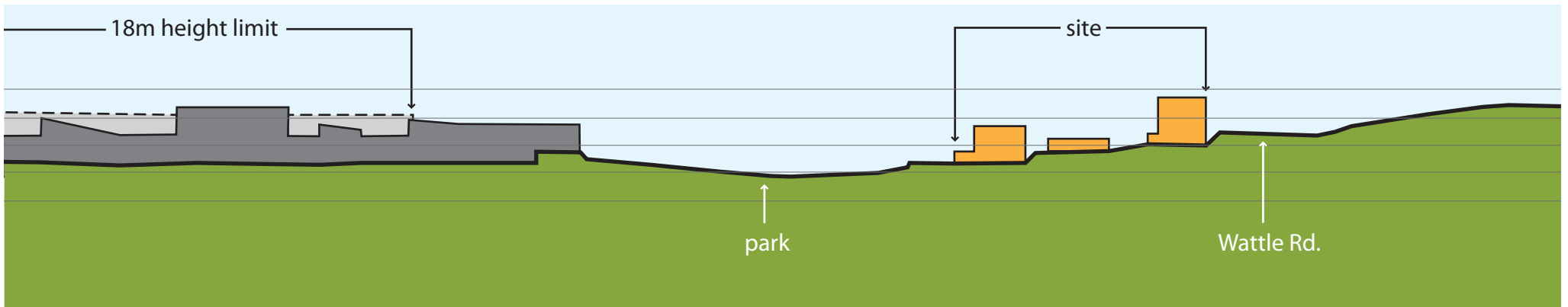
In this context, the urban form is generally 3-4 storeys with some limited 5 storey and taller development along the College Street ridge (RL 50m) and at the focal points.

In a North-South direction, the City Centre extends from New Lake Entrance Road, in the north to Benson Avenue / Cygnet Avenue in the south.

In this context the City Centre contains elevated 2 storey buildings along New Lake Entrance Road, rises to 5-8 storey at the Knoll, and holds at 4-5 storeys for proposed new Council building at Cygnet Avenue.

Thus a 4 storey development to Wattle Road is entirely appropriate, as are 4 storey buildings to the Park Edge at Minga Avenue. Note that building heights should be translated into metres for the LEP/DCP and this would suggest 15m to Wattle Road ( $4 \times 3 = 12\text{m} + \text{roof } 3\text{m} = 15\text{m}$ ). Note that the slope should permit a building of 18m at the internal side of this building. This is consistent with City Centre existing height limits in the LEP of 15m with an 18m at gateway sites (the subject site is certainly a gateway to the City Centre).







Uniting Care Aging

# Shellharbour Seniors Precinct Development Strategy

Annard & Associates Urban Design & Integrated Design Group  
February 2012